



**Armidale Dumaresq
Development Control Plan 2012**

Part 6 Locality Specific Precincts

Chapter 6.2 Duval Precinct

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Part 1 General Provisions

1.1 Introduction

The Duval Precinct Chapter has been developed to provide further detail in relation to the future development of land shown on the Duval Strategy Plan Map (see Appendix 1).

The purpose of this chapter is to identify site specific strategies and requirements that must be incorporated into future development of land in the Duval Precinct.

The provisions of this chapter should be read in conjunction with other relevant chapters of the Armidale Dumaresq Development Control Plan 2012.

1.2 Objectives

The objective of this chapter is to ensure that development of urban areas involving multiple land holders is carried out in a coordinated and integrated manner.

1.3 Addressing the guidelines in this chapter

Additional guidelines specifically for land in the Duval Precinct are set out in this chapter. These are expressed in the form of objectives that need to be addressed for each development proposal. For each objective (O), 'acceptable solutions' (S) are provided which, if met, will ensure compliance. Alternative approaches may be proposed, provided these adequately address the relevant objectives and comply with legislation.

1.4 Land to which this chapter applies

The Duval Precinct chapter applies to land in the area bound by Niagara Street, Cluny Road, New England Highway, Crest Road and Erskine Street as shown below in Figure 1.

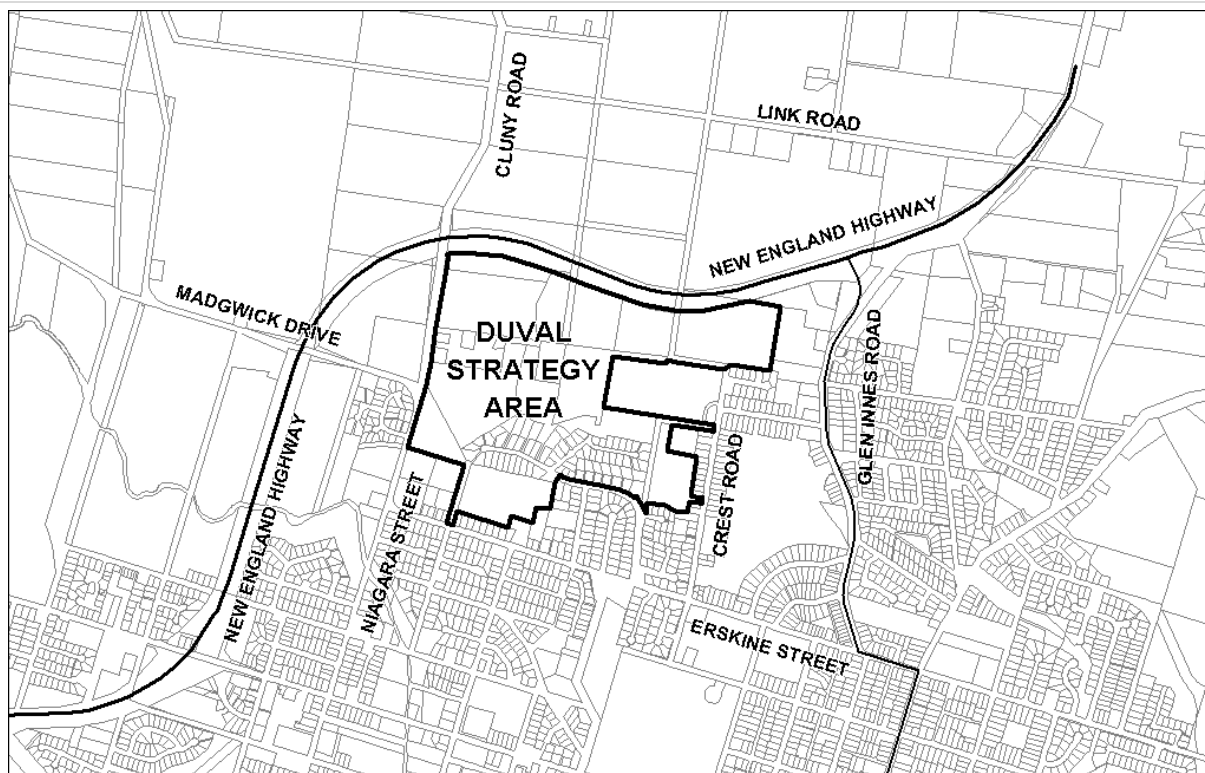


Figure 1 Duval Precinct Area

Part 2 Requirements for development in the Duval Precinct

2.1 Road and access networks

- S.1 The road layout and inter-allotment connections shown on the Strategy Plan are diagrammatic only and provide an outline of the desired network of road, pedestrian, bicycle and inter-allotment connections for the area. The exact location of roads, pedestrian and bicycle networks and other access ways will be subject to subdivision and engineering design.
- S.2 Subdivision design should minimise the visual impact of roads on the local environment by endeavouring to preserve existing trees and by following contour lines as closely as possible.

2.2 Site specific requirements

The following requirements apply to specific locations shown with corresponding circled numbers on the Duval Strategy Plan.

① Northcott Street Road Reserve (north of Erskine Street)

Objectives

- O.1 To protect and maintain the Northcott Street road reserve as a pedestrian access route.
- S.1 The Northcott Street road reserve shall be retained to provide future pedestrian access to Lot 25 DP 531103, Lot 86 DP 865309 and to the western end of Ash Tree Drive.
- S.2 The road reserve shall also be retained for vehicle access to Lot 25 DP 531103 and possibly Lot 86 DP 865309. Vehicle access via the Northcott Street road reserve to Lot 86 DP 865309 will involve bridging the watercourse and possible widening, as required, at the expense of the developer.
- S.3 Applications for any significant development proposing the use of Northcott Street for vehicle access shall include a Traffic Study to assess the need for upgrading the intersection of Northcott and Erskine Streets and to demonstrate that the road design will deter the use of this route as a 'short cut' between Erskine Street and the eastern end of Madgwick Drive.

② Road Reserve north of Ash Tree Drive to Munro Street

The road reserve between Ash Tree Drive and Munro Street is identified in the *Armidale Greening Plan* as a potential wildlife corridor.

Objectives

- O.1 To protect and maintain the road reserve north of Ash Tree Drive to Munro Street as a wildlife corridor and pedestrian access route.
- S.1 This road reserve shall be retained to provide:
 - shared vehicular and pedestrian access over its northern section between Munro Street and the existing dwelling on Lot 42 DP 794031 (if required), subject to costs being met by the beneficiary land holder;
 - pedestrian access over the southern section of road reserve to Ash Tree Drive; and
 - embellishment and revegetation where appropriate to enhance its function as a wildlife corridor.
- S.2 The proposed pedestrian link from Ash Tree Drive to Lot 42 DP 794031 shall provide for all-weather access.

③ Munro Street Road Reserve

Objectives

- O.1 To maintain the Munro Street road reserve for the multiple uses outlined below.

- S.1 This road reserve is to be maintained to provide access to the ABC Transmitter site, Lot 42 DP 794031, and to provide pedestrian (and possibly emergency vehicle) access between proposed Golden Grove and Crest Road. The road may also be used for future additional access to Duval High School.
- S.2 Any extensions or upgrading to the road carriageway in the event of significant development shall be at the cost of the developer(s).

④ Scholes Street road reserve

Scholes Street road reserve is identified in the *Armidale Greening Plan* as a potential wildlife corridor.

Objectives

- O.1 To protect and maintain the Scholes Street road reserve as a wildlife corridor.

- S.1 This road reserve shall be retained in a primarily natural state to form a wildlife corridor with the inclusion of an all-weather pedestrian/cycleway link between Harden Street and Link Road. Access via the highway under-pass shall be designed and constructed taking into consideration AUSTRROADS Guide to Engineering Practice, the RTA NSW Bicycle Guidelines and the Armidale Bicycle Strategy and Action Plan 2012.
- S.2 No vehicular access shall be permitted across the Scholes Street road reserve corridor in order to preserve its primary function as a pedestrian/cycle route. East-west traffic flow in this vicinity shall only be permitted along Harden Street.
- S.3 Significant development of land adjacent to the corridor shall be required to provide pedestrian access as indicated on the Strategy Plan. Pedestrian links between adjacent streets and the corridor shall be provided at intervals not exceeding 100 metres.
- S.4 Development fronting the Scholes Street road reserve shall provide a 3 metre strip of land along the road reserve boundary (to be dedicated to Council) to allow for future maintenance between the property boundaries and the reserve.
- S.5 The proposed pedestrian/cycleway link shall extend westwards from Scholes Street to the land zoned RE1 Public Recreation on the western side of Duval High School.

⑤ Harden Street and pedestrian/cycleway cross-over

- S.1 Traffic calming measures and/or a pedestrian crossing shall be provided at the cross-over point between Harden Street and the Scholes Street pedestrian/cycleway path, in conjunction with the western extension of Harden Street (see ⑥).

⑥ Harden Street to Cluny Road Link

Objectives

- O.1 To form connections between residential development areas.
- S.1 Harden Street shall be extended in a westerly direction to form a connecting road between Cluny Road and Crest Road.
- S.2 The eastern section of the road marked ⑥ shall require:
 - possible road widening at the existing western end of Harden Street; and
 - traffic calming measures and/or pedestrian crossing in the vicinity of Duval High School, as outlined above in ⑤.
- S.3 The western section of proposed road ⑥ has potential to intersect with Cluny Road in a number of locations, subject to the timing of development and final subdivision design. Two potential options are shown diagrammatically on the Strategy Plan, however only one intersection shall be constructed in this locality in order to limit the number of intersections along Cluny Road.

⑦ Northern Extension of proposed Grandview Crescent								
S.1 Proposed Grandview Crescent (created via DA 0405/0238) shall be extended north through Lot 82 DP 785077 to intersect with the road identified in point ⑥ above.								
S.2 Dedication of the section of road ⑦ which traverses RE1 Public Recreation may only occur following an amendment to its status from Public Land to Operational Land (in accordance with the Local Government Act 1993).								
⑧ Intersection of roads ⑥ and ⑦								
Point ⑧ relates to the following lots:								
<table> <tr> <td>Lot 1 DP 131912</td> <td>Lot 4 DP 255035</td> </tr> <tr> <td>Lot 33 DP 593364</td> <td>Lot 7 DP 255035</td> </tr> <tr> <td>Lot 83 DP 785077</td> <td>Lot 21 DP 1168082</td> </tr> <tr> <td>Lot 1 DP 738275</td> <td></td> </tr> </table>	Lot 1 DP 131912	Lot 4 DP 255035	Lot 33 DP 593364	Lot 7 DP 255035	Lot 83 DP 785077	Lot 21 DP 1168082	Lot 1 DP 738275	
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Lot 33 DP 593364	Lot 7 DP 255035							
Lot 83 DP 785077	Lot 21 DP 1168082							
Lot 1 DP 738275								
S.1 The final design, location and construction of an intersection between proposed roads ⑥ and ⑦ will be subject to the timing of development and layout for subdivision of the above lots. Any proposal for significant development of the subject lots must make provision for access to adjacent lots generally in accordance with the Strategy Plan.								
S.2 Proposals for significant development of any of the above lots shall include a road network plan indicating a proposed road layout over the area shown on the Strategy Plan as ⑧ that achieves the inter-allotment and through road connections indicated on the Strategy Plan.								
⑨ Cluny Road/Madgwick Drive/Niagara Street intersection								
S.1 The preferred route for vehicle access/egress to/from Lot 33 DP 593364 and Lot 83 DP 785077, in the event of significant development of those properties, is via Cluny Road to the Cluny Road/Madgwick Drive/Niagara Street intersection. This route may also be required for access to Lot 86 DP 856309, depending on the timing and design of development for this land.								
S.2 Any proposal for significant development of Lot 86 DP 856309 shall include details for future access in the vicinity of the Cluny Road/Madgwick Drive/Niagara Street intersection. Proposals must demonstrate that adequate sight distances are available and that safe and efficient access can be provided to/from any development in this locality.								
⑩ New England Highway								
O.1 To manage traffic flows and noise buffers for land proximal to the New England Highway.								
O.2 To minimise any adverse visual impact on views both to and from the New England Highway.								
S.1 No access is permitted directly onto the New England Highway.								
S.2 The design and layout of roads and lots adjacent to the New England Highway shall take into consideration the impacts of any traffic related noise identified in the site analysis. Any proposed noise attenuation buffers shall generally be incorporated into the development (e.g. larger residential lots) rather than being dedicated as public land or reserve.								
S.3 Any significant development of land with frontage to the New England Highway shall include an assessment of the visual appearance of the development from the highway, and shall outline proposed measures to be incorporated into the development to minimise any adverse visual impact and integrate the appearance of the development with the surrounding landscape when viewed from the highway corridor and beyond.								
S.4 The Armidale Dumaresq Bicycle Strategy includes the proposed network routes outlined below, and as shown on the Strategy Plan. Where necessary, significant development of land								

incorporating, or adjacent to, a proposed bicycle network route shall include provisions to enable construction of the route as outlined in the Bicycle Strategy.

OR3 North Hill – UNE via Duval Street (primary on-road route)

SP11 Erskine Street – Ash tree Drive (new primary off-road route)

2.3 Bicycle network

S.1 The Armidale Dumaresq Bicycle Strategy includes the proposed network routes outlined below, and as shown on the Strategy Plan. Where necessary, significant development of land incorporating, or adjacent to, a proposed bicycle network route shall include provisions to enable construction of the route as outlined in the Bicycle Strategy.

OR3 North Hill – UNE via Duval Street (primary on-road route)

SP11 Erskine Street – Ash tree Drive (new primary off-road route)

2.4 Utility services

S.1 The supply of sewer and reticulated water to any new development shall be carried out in accordance with Council's Water Supply and Sewerage Development Servicing Plan and the relevant chapters of this DCP.

S.2 The proposed location of new sewer and water infrastructure required to service future development is outlined on the Duval Strategy Plan.

2.5 Stormwater drainage

S.3 Stormwater drainage systems shall be designed and provided in accordance with *Chapter 2.7 – Floodplain Protection and Stormwater Drainage*. In addition, development of land to which the Strategy Plan applies shall provide, where relevant, an easement or riparian area reserve over that part of the stormwater drainage system between the New England Highway and Cluny Road.

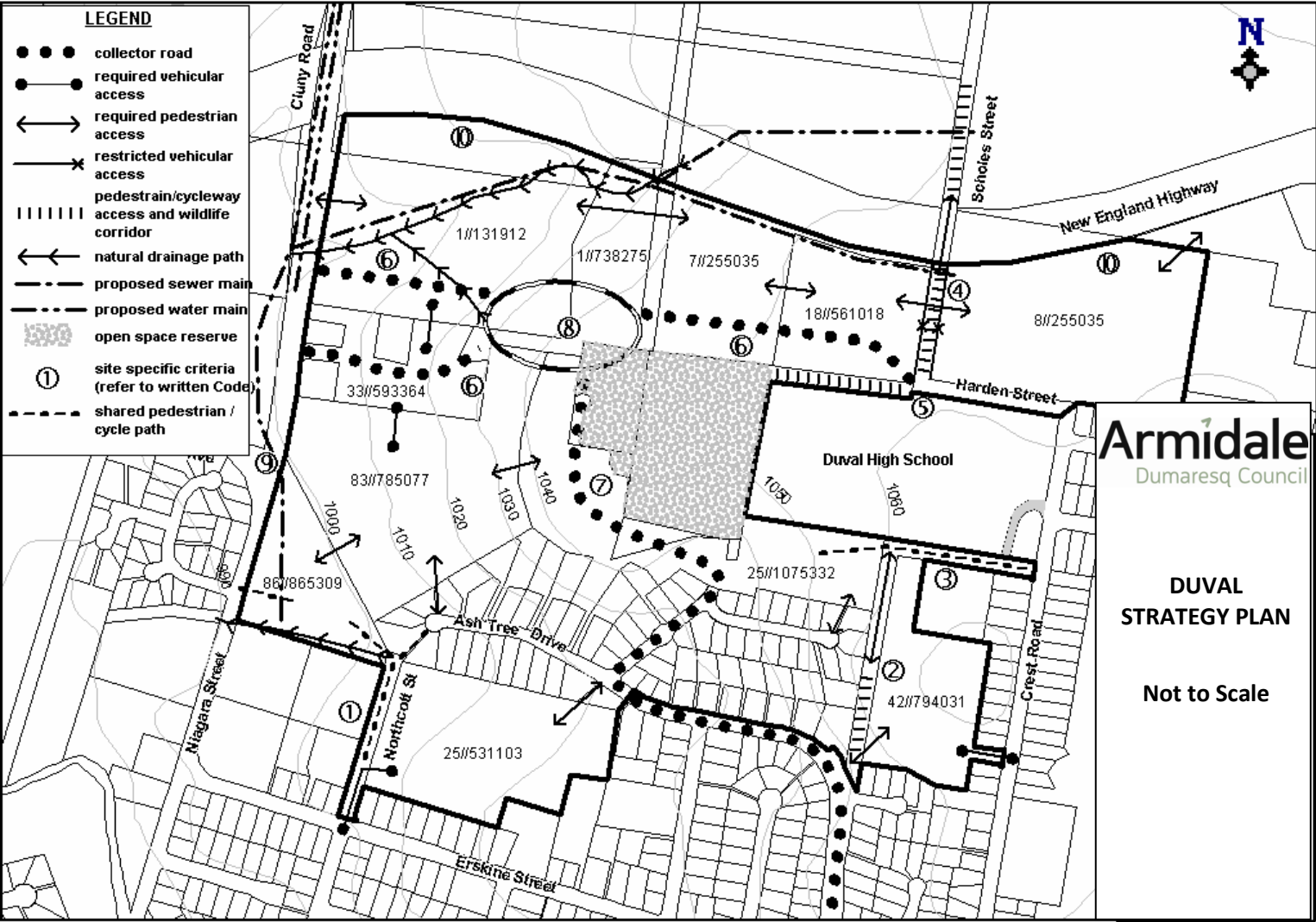


Figure 2 Duval Strategy Plan