



**Armidale Dumaresq  
Development Control Plan 2012**

**Section 6      Locality Specific Precincts**

**Chapter 6.5      Shambrook Avenue Precinct**

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## Table of Contents

Part 1	General provisions .....	1
	1.1 Introduction.....	1
	1.2 Objectives .....	1
	1.3 Addressing the guidelines in this chapter .....	1
	1.4 Land to which this chapter applies.....	1
Part 2	Requirements for development in the Shambrook Avenue Precinct.....	2
	2.1 Road and access networks .....	2
	2.2 Pedestrian and cycle access .....	2
	2.3 New England Highway .....	2
	2.4 Highway noise.....	2
	2.5 Stormwater management .....	3

## Part 1 General provisions

### 1.1 Introduction

The Shambrook Avenue Precinct Chapter has been developed to provide further detail in relation to the future development of land shown on the Shambrook Avenue Strategy Plan Map (see Figure 1).

The purpose of this chapter is to identify site specific strategies and requirements that must be incorporated into future development when development of land in the Shambrook Avenue Precinct occurs.

The provisions of this chapter should be read in conjunction with other relevant chapters of the Armidale Dumaesq Development Control Plan 2012.

### 1.2 Objectives

The objective of this chapter is to ensure that development of urban areas involving multiple land holders is carried out in a coordinated and integrated manner.

### 1.3 Addressing the guidelines in this chapter

Additional guidelines specifically for land in the Shambrook Avenue Precinct are set out in this chapter. These are expressed in the form of objectives that need to be addressed for each development proposal. For each objective (O), 'acceptable solutions' (S) are provided which, if met, will ensure compliance. Alternative approaches may be proposed, provided these adequately address the relevant objectives and comply with legislation.

### 1.4 Land to which this chapter applies

The Shambrook Avenue Strategy Plan applies to the area in heavy black bordered by Shambrook Avenue and the New England Highway shown below in Figure 1.

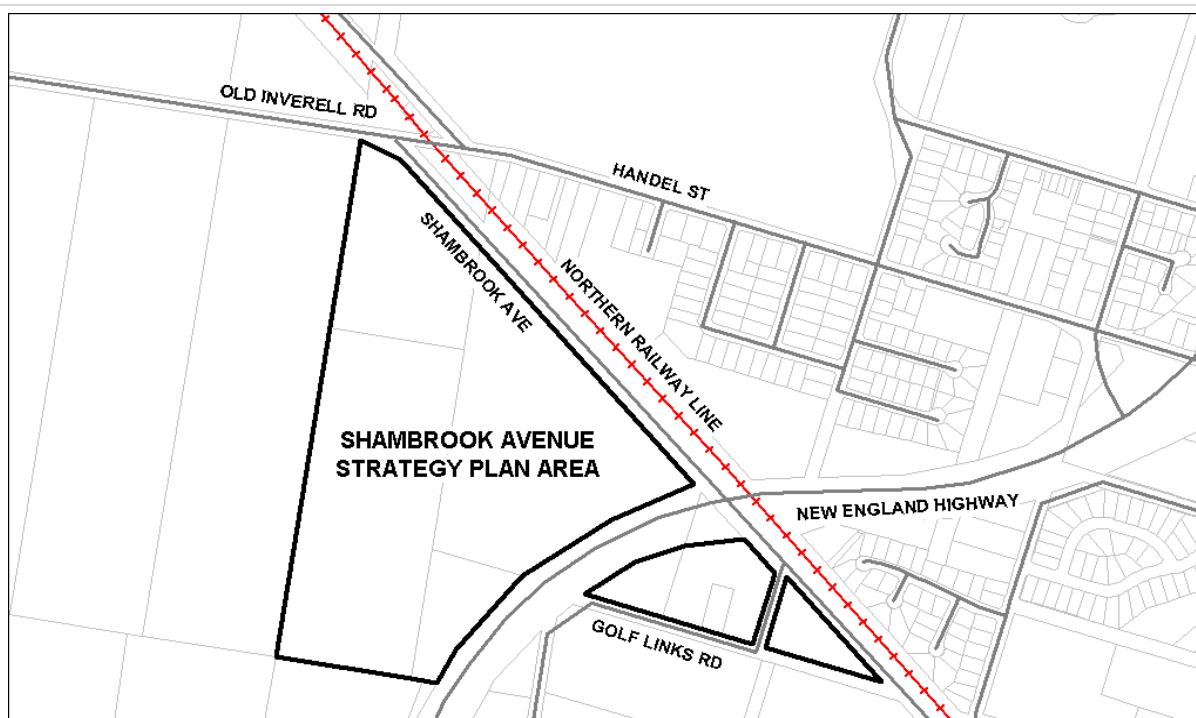


Figure 1 Shambrook Avenue Precinct Area

## Part 2 Requirements for development in the Shambrook Avenue Precinct

### 2.1 Road and access networks

- S.1 The road layout and inter-allotment connections shown on the Strategy Plan are diagrammatic only and provide an outline of the desired network of road, pedestrian, bicycle and inter-allotment connections for the area. The exact location of roads, pedestrian and bicycle networks and other access ways will be subject to subdivision and engineering design in response to the site analysis.
- S.2 Subdivision design should minimise the visual impact of roads on the local environment by endeavouring to preserve existing trees and by following contour lines as closely as possible.

### 2.2 Pedestrian and cycle access

- S.1 Shared pedestrian/cycle access shall be provided from Shambrook Avenue to existing development on the northern side of the Main Northern Railway Line in conjunction with any significant development of land to which the Strategy Plan applies.
- S.2 A number of options for pedestrian/cycle access have been identified on the Strategy Plan. The final route for any access link will be subject to negotiation with the affected landholder(s) to acquire suitable land for construction of the link, and concurrence of the relevant Rail Authority for a rail line crossing.
- S.3 The options for shared pedestrian/cycle access from Shambrook Avenue require construction of a link connecting to either:
- a) the existing walking track on Old Inverell Road/Handel Street;
  - b) the corner of Glenelg Road and Helen Avenue, via Helen Avenue Park; or
  - c) White Avenue.

### 2.3 New England Highway

- O.1 To manage traffic flows and noise buffers for land proximal to the New England Highway.
- O.2 To minimise any adverse visual impact on views both to and from the New England Highway.
- S.1 No access is permitted directly onto the New England Highway.
- S.2 The design and layout of roads and lots adjacent to the New England Highway shall take into consideration the impacts of any traffic related noise identified through the Site Analysis Plan. Any proposed noise attenuation buffers shall generally be incorporated into the development (e.g. larger residential lots) rather than being dedicated as public land or reserve.
- S.3 Proposed development of land with frontage to the New England Highway shall include an assessment of the visual appearance of the development from the highway, and shall outline proposed measures to be incorporated into the development to minimise any adverse visual impact and integrate the appearance of the development with the surrounding landscape when viewed from the highway corridor and beyond.

### 2.4 Highway noise

Properties adjacent to the New England Highway may experience traffic-generated noise. Noise impacts must be addressed in accordance with the relevant DCP Chapters, Council policy and guidelines.

**2.5 Stormwater management**

- S.1 Stormwater drainage systems shall be designed and provided in accordance with *Chapter 2.7 – Floodplain Protection and Stormwater Drainage*. The drainage concept for the Shambrook Avenue Strategy Plan area is to mitigate any potential flooding of downstream properties north of Shambrook Avenue by redirecting storm water flows to Martin’s Gully.
- S.2 In addition, and where relevant, an easement or riparian area reserve development of land upstream of the drainage discharge point into Martin’s Gully shall direct storm water via an easement or reserve to the open drain constructed along Shambrook Avenue, then through to Martin’s Gully.

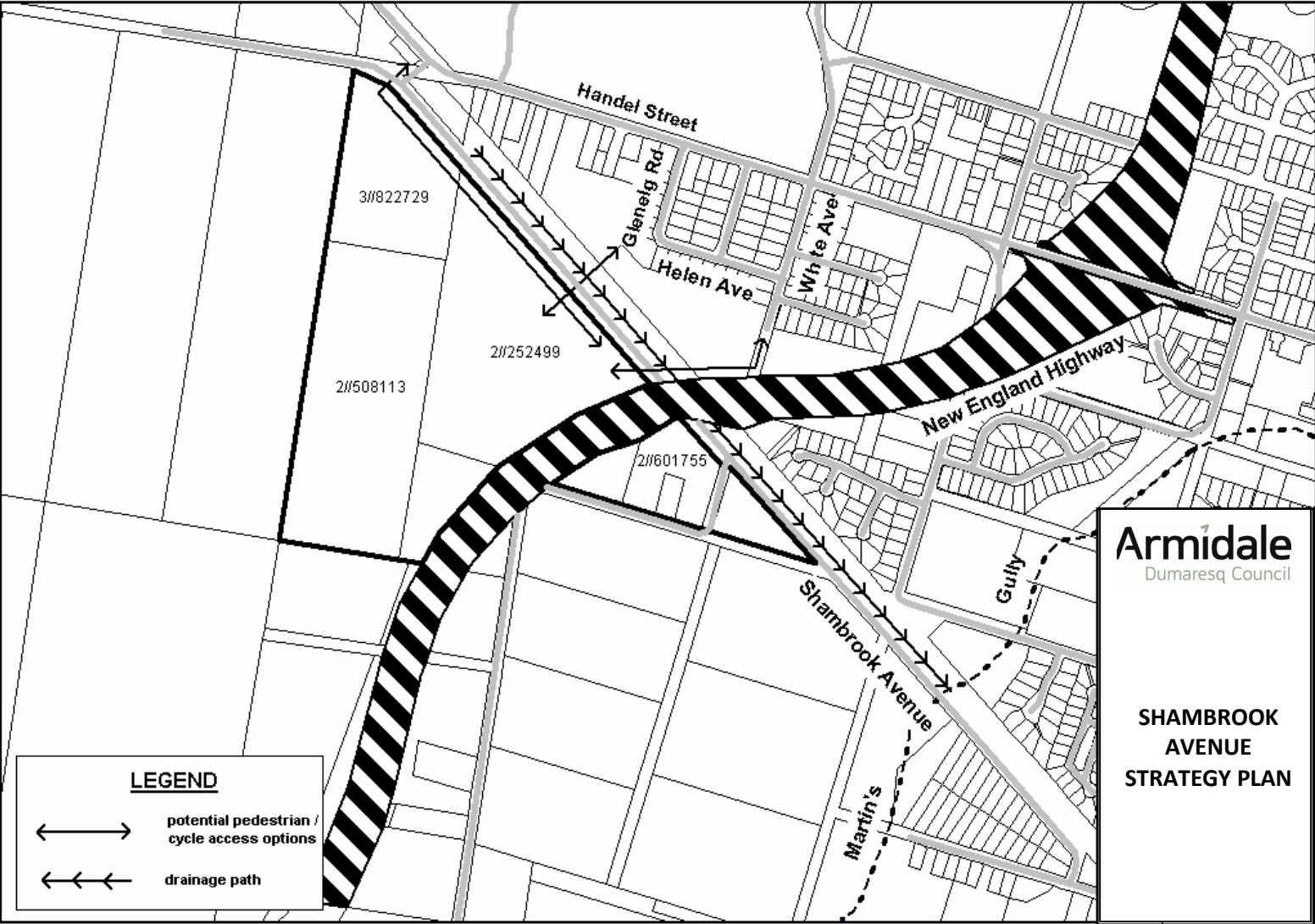


Figure 2 Shambrook Avenue Strategy Plan